

APPENDIX B
ADA/ Bicycle Compliancy Study -
Existing Sidewalk Evaluation



8000 Westpark Drive
 Tysons, Virginia 22102
 571-395-8100

Project: Rte. 17/ 29 Bus/ 211 (Broadview Ave.) - Access Management Improvements
Location: South of Frost Avenue to South of Winchester Street
Date: April, 2015

State Project Number: VDOT 7017-156-103, P101, R201, C501 UPC No: 76408
County: Fauquier
Town: Warrenton, VA

ADA/Bicycle Compliance Study - Existing Sidewalk Evaluation

| Station | Sidewalk DNE in Topo | Offset (LT/RT) | Ex. Width (ft) | Roadway Grade | SW Running Slope 5% MAX | SW X-Slope 2% MAX | ADA (Y/N) | Remarks |
|---------|----------------------|----------------|----------------|---------------|-------------------------|-------------------|-----------|---|
| 17+00 | | LT | 5.73 | -1.32% | 4.39% | 2.03% | N | |
| 18+00 | | LT | 7.84 | -1.32% | 0.35% | 1.89% | Y | |
| 20+12 | | LT | 4.00 | 1.57% | 2.08% | 3.28% | N | WB Frost Ave.; Parallel to Roadway |
| 20+42 | | RT | 4.00 | 1.57% | 0.12% | 0.73% | N | Spur SB W. Shirley Ave. To EB Waterloo St. |
| 20+59 | | RT | 4.00 | 1.57% | 3.97% | 0.15% | N | EB Waterloo St.; Parallel to Roadway |
| 20+72 | | LT | 4.00 | 1.57% | 0.12% | 2.29% | N | Pinch Point: Spur from SB Broadview Ave. to WB Frost Ave. |
| 21+18 | | RT | 4.00 | 1.57% | 0.26% | 2.23% | N | Spur WB Waterloo St to NB Broadview Ave. |
| 23+00 | | LT | 7.80 | 1.57% | 3.20% | 1.01% | Y | |
| 24+00 | | LT | 7.15 | 1.57% | 4.40% | 2.27% | N | |
| 25+00 | | LT | 7.60 | 1.57% | 0.11% | 0.12% | Y | |
| 26+00 | | LT | 8.2 | 1.57% | 2.95% | 0.50% | Y | |
| 27+25 | | LT | 7.85 | 1.57% | 1.41% | 0.08% | Y | |
| | * | RT | N/A | 1.57% | N/A | N/A | N/A | |
| 28+75 | | LT | 7.98 | 1.57% | 2.27% | 0.33% | N/A | |
| | | RT | 7.81 | 1.57% | 2.02% | 1.48% | Y | |
| 29+00 | | LT | 7.8 | 1.57% | 3.49% | 4.43% | N | |
| 29+75 | | LT | 7.7 | 1.57% | 2.33% | 5.64% | N | |
| | | RT | 7.5 | 1.57% | 1.13% | 2.61% | N | |
| 30+00 | | LT | 7.9 | 1.57% | 2.24% | 1.47% | Y | |
| | | RT | 7.8 | 1.57% | RAMP | 4.93% | N | |
| 31+50 | | LT | 7.70 | 1.57% | 0.33% | 4.48% | N | |
| | | RT | 7.48 | 1.57% | 0.72% | 5.21% | N | |
| 32+00 | * | LT | N/A | 1.57% | N/A | N/A | N/N | |
| | * | RT | N/A | 1.57% | N/A | N/A | N/A | |
| 34+00 | * | LT | N/A | 3.40% | N/A | N/A | N/A | |
| | | RT | 7.65 | 3.40% | 1.50% | 0.50% | Y | |
| 35+00 | * | LT | N/A | 3.40% | N/A | N/A | N/A | |
| | | RT | 7.9 | 3.40% | 0.95% | 0.36% | Y | |

ADA/Bicycle Compliance Study - Existing Sidewalk Evaluation

| Station | Sidewalk DNE in Topo | Offset (LT/RT) | Ex. Width (ft) | Roadway Grade | SW Running Slope 5% MAX | SW X-Slope 2% MAX | ADA (Y/N) | Remarks |
|---------|----------------------|----------------|----------------|---------------|-------------------------|-------------------|-----------|---------|
| 36+00 | | LT | 7.8 | 3.40% | 1.30% | 0.74% | Y | |
| | | RT | 7.9 | 3.40% | 3.45% | 1.32% | Y | |
| 37+50 | * | LT | N/A | 3.40% | N/A | N/A | N/A | |
| | | RT | 7.78 | 3.40% | 2.65% | 0.96% | Y | |
| 39+00 | | LT | 7.7 | 3.40% | 2.51% | 1.86% | Y | |
| | | RT | 7.6 | 3.40% | 3.99% | 1.71% | Y | |
| 40+50 | | LT | 7.89 | 3.40% | 3.83% | 3.07% | N | |
| | * | RT | N/A | 3.40% | N/A | N/A | N/A | |
| 41+50 | | LT | 7.98 | 3.40% | 8.20% | 0.11% | N | |
| | * | RT | N/A | 3.40% | N/A | N/A | N/ | |
| 42+00 | * | RT | N/A | 3.40% | N/A | N/A | N/A | |
| 42+75 | | LT | 7.99 | 3.40% | 2.05% | 2.74% | N | |
| | * | RT | N/A | 3.40% | N/A | N/A | N/A | |
| 43+50 | | LT | 7.89 | -3.37% | 2.22% | 1.56% | Y | |
| | * | RT | N/A | -3.37% | N/A | N/A | N/A | |
| 44+25 | | RT | 7.94 | -3.37% | 2.17% | 0.01% | Y | |
| 45+00 | | LT | 8.2 | -3.37% | 2.85% | 11.13% | N | |
| | | RT | 7.8 | -3.37% | 7.54% | 5.86% | N | |
| 46+25 | | LT | 7.7 | -3.37% | 2.09% | 1.58% | Y | |
| 47+00 | | LT | 7.7 | -3.37% | 5.91% | 2.41% | N | |
| | * | RT | N/A | -3.37% | N/A | N/A | N/A | |
| 48+00 | | LT | 7.9 | -3.37% | 1.64% | 2.15% | N | |
| 49+00 | | RT | 8.1 | -3.37% | 2.00% | 2.30% | N | |
| 49+50 | | LT | 8.2 | -3.37% | 2.07% | 2.24% | N | |
| 50+00 | | RT | 8.0 | -3.37% | 3.00% | 2.01% | N | |
| | | LT | 7.96 | -3.37% | 2.51% | 1.21% | Y | |
| 50+75 | | RT | 7.84 | -3.37% | 2.04% | 1.91% | Y | |
| | | LT | 8.06 | -1.75% | 3.27% | 0.72% | Y | |
| 51+75 | | RT | 8.00 | -1.75% | 1.07% | 5.93% | N | |
| | | LT | 7.49 | -1.75% | 2.00% | 3.69% | N | |
| 52+50 | | LT | 7.49 | -1.75% | 2.00% | 3.69% | N | |
| | | RT | 7.8 | -1.75% | 2.07% | 1.85% | Y | |
| 53+50 | | RT | 7.8 | -1.75% | 2.94% | 0.88% | Y | |
| | | LT | 7.3 | -1.75% | 1.19% | 0.65% | Y | |
| 54+50 | | RT | 7.7 | -1.75% | 0.86% | 0.87% | Y | |
| | | LT | 7.9 | -1.75% | 1.42% | 2.72% | Y | |
| 55+50 | | RT | 8.8 | -1.75% | RAMP | 1.68% | Y | |

ADA/Bicycle Compliance Study - Existing Sidewalk Evaluation

| Station | Sidewalk DNE in Topo | Offset (LT/RT) | Ex. Width (ft) | Roadway Grade | SW Running Slope 5% MAX | SW X-Slope 2% MAX | ADA (Y/N) | Remarks |
|---------|----------------------------|-------------------|----------------|------------------|----------------------------|----------------------|-----------|---------|
| 56+25 | | LT | 7.75 | -1.75% | 2.19% | 1.98% | Y | |
| | | RT | 7.75 | -1.75% | 1.32% | 2.01% | Y | |
| 57+00 | | RT | 7.8 | -1.75% | RAMP | 1.64% | Y | |
| 57+50 | | LT | 7.47 | -1.75% | 1.74% | 2.33% | N | |
| 58+00 | | RT | 7.7 | -1.75% | 2.70% | 0.45% | Y | |
| 59+00 | | LT | 7.8 | -1.75% | 1.36% | 1.21% | Y | |
| | | RT | 7.8 | -1.75% | 0.26% | 2.60% | N | |
| 60+00 | | LT | 6.4 | 2.10% | 0.40% | 0.53% | Y | |
| | | RT | 7.8 | 2.10% | 1.17% | 2.20% | N | |
| 61+25 | | LT | 8.00 | 2.10% | 0.53% | 4.25% | N | |
| | | RT | 8.10 | 2.10% | 0.52% | 2.39% | N | |
| 62+00 | | LT | 7.8 | 2.10% | 1.00% | 2.79% | N | |
| | | RT | 8.9 | 2.10% | 0.45% | 1.82% | Y | |
| 63+25 | | LT | 7.08 | 2.10% | 1.04% | 2.02% | N | |
| | | RT | 7.86 | 2.10% | 1.57% | 4.53% | N | |
| 64+00 | | LT | 7.9 | 2.10% | 0.21% | 4.62% | N | |
| | | RT | 8.7 | 2.10% | 0.49% | 2.43% | N | |
| 65+00 | | LT | 7.7 | -2.81% | 1.97% | 0.68% | Y | |

APPENDIX C
ADA/ Bicycle Compliancy Study –
Existing Curb Ramps Evaluation



Project: Rte. 17/ 29 Bus/ 211 (Broadview Ave.) - Access Management Improvements
Location: South of Frost Avenue to South of Winchester Street
Date: April, 2015

State Project Number: VDOT 7017-156-103, P101, R201, C501 UPC No: 76408
County: Fauquier
Town: Warrenton, VA

ADA/Bicycle Compliance Study - Existing Curb Ramps Evaluation

| Curb Ramp ID | Station | Offset (LT/RT) | Curb Ramp Type | Cross Slope | | Running Slope | | ADA Features | | | | Overall Compliance | Remarks |
|--------------|----------|----------------|----------------|------------------|-----------|----------------------------|-----------|--------------|-------------------------|--|-----------|--------------------|---|
| | | | | Ramp X-Slope (%) | ADA (Y/N) | Ramp Grade (%) 12:1 Max | ADA (Y/N) | DWS (Y/N) | Min. Landing Area (Y/N) | Running Slope Landing Area (%) 2% Max | ADA (Y/N) | ADA (Y/N) | |
| 1 | 21+14.99 | 315.49 RT | Parallel | 0.11% | Y | 3.39% | Y | N | Y | 3.55% | N | N | |
| 2 | 20+30.99 | 87.13 RT | Parallel | 3.27% | N | 5.70% | Y | N | Y | 8.47% | N | N | |
| 3 | 20+44.18 | 74.04 RT | Perp. | 2.44% | N | 8.66% | N | Y | Y | 0.31% | Y | N | |
| 4 | 20+33.81 | 44.00 RT | Perp. | 1.50% | Y | 7.10% | Y | Y | Y | N/A | N/A | N | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data. |
| 5 | 20+43.53 | 51.60 RT | Perp. | 1.05% | Y | 10.79% | N | Y | Y | 1.18% | Y | N | |
| 6 | 21+05.87 | 44.87 RT | Perp. | 0.86% | Y | 5.61% | Y | N | Y | 0.36% | Y | N | |
| 7 | 21+29.28 | 66.45 RT | Perp. | 0.87% | Y | 4.36% | Y | Y | Y | 1.04% | Y | Y | |
| 8 | 21+49.59 | 77.89 RT | Perp. | 0.33% | Y | 6.62% | Y | N | Y | 4.72% | N | N | Currently an Asphalt ramp |
| 9 | 21+88.02 | 350.10 RT | Perp. | 4.65% | N | 9.69% | N | Y | Y | 3.34% | N | N | |
| 10 | 26+63.30 | 54.32 RT | Parallel | 1.40% | Y | N/A | N/A | Y | Y | 3.20% | N | N | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data. |
| 11 | 27+50.06 | 53.06 RT | Parallel | N/A | N/A | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 12 | 28+10.55 | 58.13 RT | Parallel | N/A | N/A | 2.60% | Y | Y | Y | N/A | N/A | N/A | |
| 13 | 28+86.65 | 57.60 RT | Parallel | 2.55% | Y | 2.65% | Y | N | Y | 1.77% | Y | N | |
| 14 | 29+42.60 | 59.59 RT | Perp. | 2.89% | Y | 2.94% | Y | N | Y | 1.92% | Y | N | |
| 15 | 30+05.39 | 56.68 RT | Perp. | 6.86% | N | 10.77% | N | N | Y | 0.14% | Y | N | |
| 16 | 30+42.70 | 58.72 RT | Perp. | 2.97% | N | 8.87% | N | N | Y | 3.48% | N | N | |
| 17 | 30+73.61 | 58.47 RT | Parallel | 1.64% | Y | 2.39% | Y | N | Y | 2.35% | N | N | |
| 18 | 31+03.23 | 58.25 RT | Perp. | 2.42% | N | 5.07% | Y | N | Y | 1.49% | Y | N | |
| 19 | 32+05.04 | 57.16 RT | Parallel | 5.30% | N/A | N/A | N/A | Y | Y | 2.80% | N | N | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data. |
| 20 | 32+17.87 | 57.02 RT | Parallel | 2.90% | N | N/A | N/A | Y | Y | 10.10% | N/A | N/A | |
| 21 | 32+95.46 | 58.61 RT | Perp. | 1.64% | Y | 4.53% | Y | N | Y | 3.91% | N | N | |
| 22 | 33+34.12 | 58.60 RT | Perp. | 5.97% | N | 10.08% | N | N | Y | 3.41% | N | N | |
| 23 | 34+26.10 | 58.69 RT | Parallel | 1.21% | Y | 1.78% | Y | N | Y | 0.29% | Y | N | |
| 24 | 34+73.72 | 58.78 RT | Perp. | 5.35% | N | 4.88% | Y | N | Y | 1.75% | Y | N | |
| 25 | 35+23.34 | 58.75 RT | Perp. | 1.54% | Y | 7.97% | Y | N | Y | 3.13% | N | N | |
| 26 | 35+58.83 | 58.91 RT | Perp. | 1.54% | Y | 10.88% | N | N | Y | 3.31% | N | N | |
| 27 | 36+83.32 | 59.33 RT | Parallel | 0.88% | Y | 2.13% | Y | N | Y | 2.21% | N | N | |
| 28 | 37+32.68 | 59.76 RT | Perp. | 2.91% | N | 7.50% | Y | N | Y | 2.57% | N | N | |
| 29 | 37+72.25 | 59.73 RT | Perp. | 1.10% | Y | 9.62% | N | N | Y | 2.60% | N | N | |
| 30 | 38+00.89 | 56.84 RT | Perp. | 2.33% | N | 2.66% | Y | N | Y | 2.06% | N | N | |
| 31 | 39+76.03 | 56.20 RT | Perp. | 1.70% | Y | 2.30% | Y | Y | Y | 2.86% | N | N | |
| 32 | 40+22.00 | 51.77 RT | Perp. | 4.00% | N | 7.70% | Y | Y | Y | N/A | N/A | N/A | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data. |
| 33 | 40+91.19 | 52.93 RT | Perp. | 0.80% | Y | N/A | N/A | Y | Y | 3.70% | N | N/A | |
| 34 | 41+21.73 | 51.79 RT | Parallel | 1.30% | Y | N/A | N/A | Y | Y | 2.10% | N | N/A | |
| 35 | 42+16.97 | 53.83 RT | Parallel | 1.80% | Y | N/A | N/A | Y | Y | 1.70% | Y | N/A | |
| 36 | 42+51.06 | 53.27 RT | Perp. | 1.10% | Y | N/A | N/A | Y | Y | 2.40% | N | N/A | |
| 37 | 42+90.91 | 52.36 RT | Parallel | 2.80% | N | N/A | N/A | Y | Y | 1.00% | Y | N/A | |
| 38 | 43+15.66 | 52.57 RT | Parallel | 3.20% | N | N/A | N/A | Y | Y | 0.60% | Y | N/A | |
| 39 | 43+70.00 | 51.97 RT | Parallel | 0.40% | Y | N/A | N/A | Y | Y | 0.20% | Y | N/A | |
| 40 | 44+11.89 | 52.78 RT | Parallel | 3.22% | N | 2.52% | Y | N | Y | 0.96% | Y | N | |

ADA/Bicycle Compliance Study - Existing Curb Ramps Evaluation

| Curb Ramp ID | Station | Offset (LT/RT) | Curb Ramp Type | Cross Slope | | Running Slope | | ADA Features | | | | Overall Compliance | Remarks |
|--------------|----------|----------------|----------------|------------------|-----------|----------------------------|-----------|--------------|-------------------------|--|-----------|--------------------|--|
| | | | | Ramp X-Slope (%) | ADA (Y/N) | Ramp Grade (%) 12:1 Max | ADA (Y/N) | DWS (Y/N) | Min. Landing Area (Y/N) | Running Slope Landing Area (%) 2% Max | ADA (Y/N) | ADA (Y/N) | |
| 41 | 44+53.36 | 52.69 RT | Perp. | 2.72% | N | 3.75% | Y | N | Y | 1.89% | Y | N | |
| 42 | 44+76.68 | 52.80 RT | Parallel | 1.07% | Y | 5.33% | Y | N | Y | 6.41% | N | N | |
| 43 | 45+21.35 | 59.59 RT | Parallel | 4.26% | N | 7.20% | Y | N | Y | 6.96% | N | N | |
| 44 | 45+53.76 | 59.16 RT | Parallel | 6.77% | N | 2.33% | Y | N | Y | 2.51% | N | N | |
| 45 | 45+72.72 | 59.33 RT | Parallel | 6.17% | N | 2.47% | Y | N | Y | 3.26% | N | N | |
| 46 | 47+52.11 | 46.30 RT | Parallel | 1.57% | Y | FLAT | Y | N | Y | N/A | N | N | |
| 47 | 47+68.66 | 47.27 RT | Perp. | 1.56% | Y | 11.37% | N | Y | Y | FLAT | Y | N | |
| 48 | 48+18.30 | 58.49 RT | Perp. | 6.42% | N | 3.37% | Y | N | Y | 1.73% | Y | N | |
| 49 | 48+48.92 | 58.99 RT | Perp. | 6.09% | N | 5.61% | Y | N | Y | 1.75% | Y | N | |
| 50 | 48+79.72 | 59.28 RT | Perp. | 2.04% | N | 10.77% | N | N | Y | 2.40% | N | N | |
| 51 | 49+33.40 | 59.38 RT | Perp. | 2.58% | N | 12.44% | N | N | Y | 2.06% | N | N | |
| 52 | 49+64.33 | 59.47 RT | Perp. | 2.35% | N | 5.21% | Y | N | Y | 3.10% | N | N | |
| 53 | 50+11.91 | 59.49 RT | Perp. | 0.54% | Y | 6.27% | Y | N | Y | 4.71% | N | N | |
| 54 | 50+54.65 | 59.30 RT | Parallel | 2.79% | N | 1.03% | Y | N | Y | 0.77% | Y | N | |
| 55 | 50+83.85 | 59.63 RT | Perp. | 0.39% | Y | 7.67% | Y | N | Y | 5.70% | N | N | |
| 56 | 51+37.85 | 59.39 RT | Parallel | 0.61% | Y | 2.64% | Y | N | Y | 1.83% | Y | N | |
| 57 | 52+08.53 | 59.29 RT | Perp. | 0.74% | Y | 6.35% | Y | N | Y | 1.17% | Y | N | |
| 58 | 52+57.30 | 59.06 RT | Parallel | 1.66% | Y | 3.40% | Y | N | Y | 1.77% | Y | N | |
| 59 | 52+91.65 | 58.68 RT | Parallel | 2.75% | N | 3.40% | Y | N | Y | 2.98% | N | N | |
| 60 | 53+23.90 | 59.21 RT | Parallel | 4.57% | N | 4.61% | Y | N | Y | 3.39% | Y | N | |
| 61 | 53+56.60 | 58.43 RT | Perp. | 0.05% | Y | 6.39% | Y | N | Y | 4.61% | N | N | |
| 62 | 54+06.65 | 59.12 RT | Parallel | 4.21% | N | 1.30% | Y | N | Y | 1.30% | Y | N | |
| 63 | 54+82.46 | 59.24 RT | Perp. | 1.99% | Y | 1.46% | Y | N | Y | 1.15% | Y | N | |
| 64 | 55+33.68 | 57.00 RT | Perp. | 2.28% | N | 2.99% | Y | N | Y | 2.20% | N | N | |
| 65 | 55+53.91 | 55.97 RT | Perp. | 1.51% | Y | 9.05% | N | N | Y | 2.20% | N | N | |
| 66 | 55+87.91 | 57.52 RT | Perp. | 0.37% | Y | 2.40% | Y | N | Y | 2.14% | N | N | |
| 67 | 56+60.79 | 57.35 RT | Perp. | 1.95% | Y | 3.48% | Y | N | Y | 1.56% | Y | N | |
| 68 | 56+92.61 | 56.87 RT | Parallel | 2.09% | N | 1.25% | Y | N | Y | 0.85% | Y | N | |
| 69 | 57+27.68 | 56.96 RT | Parallel | 1.11% | Y | 2.24% | Y | N | Y | 1.93% | Y | N | |
| 70 | 57+60.31 | 57.22 RT | Parallel | 0.49% | Y | 2.16% | Y | N | Y | 1.19% | Y | N | |
| 71 | 60+36.74 | 56.68 RT | Perp. | 5.16% | N | 7.12% | Y | N | Y | 0.87% | Y | N | |
| 72 | 60+95.71 | 60.20 RT | Perp. | 1.19% | Y | 7.19% | Y | N | Y | 1.89% | Y | N | |
| 73 | 62+20.98 | 59.09 RT | Perp. | 5.43% | N | 5.95% | Y | N | Y | 1.82% | Y | N | |
| 74 | 62+84.81 | 59.47 RT | Perp. | 3.66% | N | 7.10% | Y | N | Y | 4.60% | N | N | |
| 75 | 64+19.62 | 61.86 RT | Perp. | 5.04% | N | 7.76% | Y | N | Y | 2.32% | N | N | |
| 76 | 17+17.88 | 53.19 LT | Perp. | 2.35% | N | 0.70% | Y | Y | Y | 6.67% | N | N | |
| 77 | 17+71.05 | 66.82 LT | Perp. | 1.47% | Y | 2.67% | Y | Y | Y | 1.64% | Y | N | |
| 78 | 19+26.10 | 70.36 LT | Perp. | N/A | N/A | N/A | N/A | Y | Y | N/A | N/A | N/A | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data |
| 79 | 19+50.00 | 63.21 LT | Perp. | N/A | N/A | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 80 | 18+84.05 | 302.40 LT | Parallel | 0.58% | Y | 2.75% | Y | Y | Y | 1.80% | Y | N | |
| 81 | 18+70.98 | 349.13 LT | Parallel | 1.08% | Y | 0.66% | Y | Y | Y | 4.26% | N | N | |
| 82 | 19+50.12 | 130.07 LT | Perp. | 2.38% | N | 1.32% | Y | Y | Y | 0.64% | Y | N | Recently reconstructed ramp, verify with updated topo |
| 83 | 19+71.24 | 108.92 LT | Perp. | N/A | N/A | N/A | N/A | N/A | Y | N/A | N/A | N/A | No sidewalk in the existing topo, recently constructed. Station and Offset locations are approx. X-slope, Ramp Grade from field data |
| 84 | 19+98.91 | 50.54 LT | Perp. | 0.60% | N/A | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 85 | 20+65.16 | 52.65 LT | Perp. | 2.37% | N | 5.20% | Y | Y | Y | 0.07% | Y | N | |
| 86 | 20+35.94 | 164.20 LT. | Perp. | 2.34% | N | 10.22% | N | Y | Y | 5.01% | N | N | |
| 87 | 20+43.64 | 179.71 LT | Perp. | 1.75% | Y | 9.02% | N | Y | Y | 1.26% | Y | N | |
| 88 | 19+75.42 | 333.16 LT | Perp. | 3.02% | N | 4.55% | Y | N | Y | 1.51% | Y | N | Sidewalk not 5' width |
| 89 | 20+75.80 | 44.59 LT | Perp. | 1.59% | Y | 5.74% | Y | Y | Y | 1.34% | Y | Y | |

ADA/Bicycle Compliance Study - Existing Curb Ramps Evaluation

| Curb Ramp ID | Station | Offset (LT/RT) | Curb Ramp Type | Cross Slope | | Running Slope | | ADA Features | | | | Overall Compliance | Remarks |
|--------------|----------|----------------|----------------|------------------|-----------|----------------------------|-----------|--------------|-------------------------|--|-----------|--------------------|---|
| | | | | Ramp X-Slope (%) | ADA (Y/N) | Ramp Grade (%) 12:1 Max | ADA (Y/N) | DWS (Y/N) | Min. Landing Area (Y/N) | Running Slope Landing Area (%) 2% Max | ADA (Y/N) | ADA (Y/N) | |
| 90 | 21+36.64 | 80.76 LT | Perp. | 2.18% | N | 8.94% | N | Y | Y | 3.50% | N | N | |
| 91 | 21+46.45 | 94.15 LT | Perp. | 1.80% | Y | 10.86% | N | Y | Y | 5.40% | N | N | |
| 92 | 23+29.54 | 68.85 LT | Parallel | 7.80% | N | 0.23% | Y | Y | Y | 0.46% | Y | N | |
| 93 | 23+76.54 | 61.30 LT | Perp. | 2.69% | Y | 3.31% | Y | Y | Y | 2.62% | N | N | |
| 94 | 24+20.76 | 62.32 LT | Parallel | 6.12% | N | 5.20% | Y | N | Y | 0.87% | Y | N | |
| 95 | 24+60.02 | 62.72 LT | Perp. | 3.88% | N | 6.73% | Y | N | Y | 3.71% | N | N | |
| 96 | 25+26.77 | 62.06 LT | Perp. | 3.86% | N | 8.58% | N | N | Y | 0.63% | Y | N | |
| 97 | 25+65.48 | 60.15 LT | Perp. | 1.04% | Y | 2.72% | Y | N | Y | 2.07% | N | N | |
| 98 | 26+41.07 | 63.52 LT | Parallel | 1.84% | Y | 3.48% | Y | N | Y | 2.78% | N | N | |
| 99 | 26+99.51 | 64.17 LT | Perp. | 2.10% | Y | 3.76% | Y | N | Y | 1.38% | Y | N | |
| 100 | 27+84.01 | 64.87 LT | Parallel | 4.80% | N | 2.57% | Y | N | Y | 1.80% | Y | N | |
| 101 | 28+42.04 | 63.39 LT | Perp. | 5.90% | N | 5.48% | Y | N | Y | 3.65% | N | N | |
| 102 | 29+22.92 | 63.97 LT | Parallel | 5.88% | N | 0.99% | Y | Y | Y | 1.04% | Y | N | |
| 103 | 29+53.96 | 64.26 LT | Parallel | 7.23% | N | 0.77% | Y | Y | Y | 0.87% | Y | N | |
| 104 | 30+11.54 | 64.43 LT | Parallel | 6.75% | N | 1.43% | Y | Y | Y | 0.03% | Y | N | |
| 105 | 30+42.88 | 64.39 LT | Perp. | 3.94% | N | 1.82% | Y | Y | Y | 1.40% | Y | N | |
| 106 | 30+79.36 | 64.59 LT | Parallel | 4.88% | N | 0.47% | Y | Y | Y | 0.04% | Y | N | |
| 107 | 31+28.71 | 63.12 LT | Perp. | 7.69% | N | 5.38% | Y | Y | Y | 4.48% | N | N | |
| 108 | 31+70.54 | 62.67 LT | Perp. | 7.33% | N | 6.64% | Y | Y | Y | 3.39% | N | N | |
| 109 | 31+97.65 | 52.73 LT | Parallel | 3.50% | N | N/A | N/A | Y | Y | N/A | N/A | N/A | No sidewalk in the existing topo. Station and Offset locations are approx. X-slope, Ramp Grade from field data. |
| 110 | 32+88.39 | 52.73 LT | Parallel | 4.10% | N | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 111 | 33+26.21 | 53.53 LT | Parallel | 2.60% | N | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 112 | 34+48.69 | 53.63 LT | Parallel | 0.30% | Y | N/A | N/A | Y | Y | N/A | N/A | N/A | |
| 113 | 34+89.12 | 53.00 LT | Perp. | 0.28% | Y | 8.30% | Y | Y | Y | 6.52% | N | N | |
| 114 | 35+19.62 | 50.91 LT | Perp. | 11.96% | N | 9.74% | N | Y | Y | 11.37% | N | N | |
| 115 | 35+69.89 | 57.00 LT | Perp. | 6.91% | N | 3.78% | Y | Y | Y | 1.59% | Y | N | |
| 116 | 36+70.96 | 55.47 LT | Parallel | 0.50% | Y | 2.93% | Y | Y | Y | 2.31% | N | N | |
| 117 | 37+83.90 | 63.06 LT | Parallel | 3.86% | N | 1.48% | Y | N | Y | 1.87% | Y | N | |
| 118 | 38+05.40 | 64.82 LT | Perp. | 0.66% | N | 7.02% | Y | Y | Y | 0.81% | Y | N | |
| 119 | 38+41.10 | 68.11 LT | Parallel | 2.70% | N | 4.95% | Y | Y | Y | 4.94% | N | N | |
| 120 | 39+77.13 | 61.24 LT | Parallel | 0.60% | Y | 2.56% | Y | Y | Y | 2.56% | N | N | |
| 121 | 39+98.60 | 61.14 LT | Parallel | 1.96% | Y | 3.51% | Y | Y | Y | 3.11% | N | N | |
| 122 | 40+66.76 | 60.20 LT | Parallel | 3.73% | N | 5.31% | Y | Y | Y | 4.96% | N | N | |
| 123 | 41+09.28 | 62.02 LT | Perp. | 0.27% | Y | 9.07% | N | Y | Y | 3.85% | N | N | |
| 124 | 41+75.56 | 61.60 LT | Perp. | 0.32% | Y | 8.35% | N | Y | Y | 1.80% | Y | N | |
| 125 | 42+06.61 | 61.21 LT | Perp. | 6.30% | N | 3.98% | Y | Y | Y | 1.58% | Y | N | |
| 126 | 43+01.59 | 62.74 LT | Parallel | 4.70% | N | 3.04% | Y | Y | Y | 2.60% | N | N | |
| 127 | 43+22.88 | 62.98 LT | Parallel | 5.01% | N | 8.37% | N | Y | Y | 7.90% | N | N | |
| 128 | 43+98.47 | 62.86 LT | Perp. | 0.84% | Y | 7.76% | Y | Y | Y | 6.08% | N | N | |
| 129 | 44+52.01 | 63.03 LT | Perp. | 6.38% | N | 8.33% | Y | Y | Y | 2.40% | N | N | |
| 130 | 45+34.39 | 63.22 LT | Perp. | 4.40% | N | 13.80% | N | N | Y | 4.09% | N | N | |
| 131 | 45+91.95 | 61.50 LT | Perp. | 1.32% | N | 8.00% | Y | N | Y | 4.04% | N | N | |
| 132 | 47+13.09 | 61.40 LT | Parallel | 0.06% | Y | 5.80% | Y | Y | Y | 3.13% | N | Y | |
| 133 | 47+70.87 | 60.20 LT | Parallel | 1.36% | Y | 1.70% | Y | Y | Y | 1.70% | Y | Y | |
| 134 | 48+65.47 | 61.60 LT | Perp. | 5.46% | N | 7.59% | Y | Y | Y | 3.95% | N | N | |
| 135 | 49+04.02 | 62.00 LT | Perp. | 4.77% | N | 1.69% | Y | Y | Y | 1.12% | Y | N | |
| 136 | 49+83.22 | 61.66 LT | Perp. | 5.55% | N | 8.92% | N | Y | Y | 6.94% | N | N | |
| 137 | 50+09.61 | 61.71 LT | Parallel | 10.83% | N | 1.02% | Y | Y | Y | 1.79% | Y | N | |
| 138 | 50+98.72 | 61.34 LT | Perp. | 3.27% | N | 9.15% | N | N | Y | 6.03% | N | N | |

ADA/Bicycle Compliance Study - Existing Curb Ramps Evaluation

| Curb Ramp ID | Station | Offset (LT/RT) | Curb Ramp Type | Cross Slope | | Running Slope | | ADA Features | | | | Overall Compliance | Remarks |
|--------------|----------|----------------|----------------|------------------|-----------|----------------------------|-----------|--------------|-------------------------|--|-----------|--------------------|---------|
| | | | | Ramp X-Slope (%) | ADA (Y/N) | Ramp Grade (%) 12:1 Max | ADA (Y/N) | DWS (Y/N) | Min. Landing Area (Y/N) | Running Slope Landing Area (%) 2% Max | ADA (Y/N) | ADA (Y/N) | |
| 139 | 51+29.71 | 61.18 LT | Parallel | 3.83% | N | 3.77% | Y | N | Y | 4.04% | N | N | |
| 140 | 52+07.41 | 59.43 LT | Perp. | 6.01% | N | 11.42% | N | N | Y | 7.45% | N | N | |
| 141 | 52+38.39 | 53.38 LT | Perp. | 4.57% | N | 3.48% | Y | N | Y | 2.10% | N | N | |
| 142 | 52+75.36 | 53.03 LT | Perp. | 1.84% | Y | 7.07% | Y | N | Y | 8.13% | N | N | |
| 143 | 53+25.39 | 56.62 LT | Perp. | 2.54% | N | 13.40% | N | N | Y | 1.40% | Y | N | |
| 144 | 53+76.68 | 56.76 LT | Perp. | 0.66% | Y | 14.44% | N | N | Y | 3.00% | N | N | |
| 145 | 54+23.33 | 60.35 LT | Parallel | 6.12% | N | 2.27% | Y | N | Y | 3.01% | N | N | |
| 146 | 54+71.11 | 60.90 LT | Parallel | 1.28% | Y | 1.84% | Y | N | Y | 1.83% | Y | N | |
| 147 | 55+11.21 | 62.58 LT | Parallel | 2.80% | N | 0.48% | Y | N | Y | 0.48% | Y | N | |
| 148 | 55+74.49 | 62.04 LT | Perp. | 9.48% | N | 2.00% | Y | N | Y | 2.00% | Y | N | |
| 149 | 56+15.03 | 61.83 LT | Perp. | 7.50% | N | 3.01% | Y | N | Y | FLAT | Y | N | |
| 150 | 56+66.26 | 63.66 LT | Perp. | 4.60% | N | 6.78% | Y | N | Y | FLAT | Y | N | |
| 151 | 57+18.89 | 64.32 LT | Parallel | 1.97% | Y | 1.41% | Y | N | Y | 1.84% | Y | N | |
| 152 | 57+73.64 | 63.86 LT | Perp. | 0.40% | Y | 8.23% | Y | N | Y | 1.58% | Y | N | |
| 153 | 58+21.46 | 64.67 LT | Parallel | 3.34% | N | 2.44% | Y | N | Y | 0.75% | Y | N | |
| 154 | 59+41.04 | 47.38 LT | Parallel | 1.66% | Y | 2.08% | Y | N | Y | 2.01% | N | N | |
| 155 | 59+83.67 | 47.38 LT | Perp. | 2.21% | N | 0.80% | Y | N | Y | 1.64% | Y | N | |
| 156 | 60+52.66 | 50.36 LT | Perp. | 5.07% | N | 9.75% | N | N | Y | 2.90% | N | N | |
| 157 | 61+02.90 | 48.48 LT | Perp. | 1.21% | Y | 9.34% | N | N | Y | 4.05% | N | N | |
| 158 | 64+22.43 | 43.51 LT | Parallel | 5.64% | N | 1.88% | Y | N | Y | 1.89% | Y | N | |
| 159 | 64+73.53 | 40.50 LT | Perp. | 4.50% | N | 4.90% | Y | N | Y | 2.50% | N | N | |

APPENDIX D
ADA/ Bicycle Compliancy Study –
Existing Commercial Entrances Evaluation



Project: Rte. 17/ 29 Bus/ 211 (Broadview Ave.) - Access Management Improvements
 Location: South of Frost Avenue to South of Winchester Street
 Date: April, 2015

State Project Number: VDOT 7017-156-103, P101, R201, C501 UPC No: 76408
 County: Fauquier
 Town: Warrenton, VA

ADA/Bicycle Compliance Study - Existing Commercial Entrances Evaluation

Note: Standard Single Two-Way Entrance Width Should be 30'-40' According to VDOT Road Design Manual

* Asphalt entrances include concrete gutter pan and approx. 2' concrete strip @ roadway edge

| Property # | Driveway ID | Property Owner | Business Name | Station | Offset (LT/RT) | Entrances (#) | Ex. Entrance Width(s) (FT) | Entrance Type (Asphalt*/Concrete) | Edge of Pavement | Pedestrian Walkway | | Remarks |
|------------|-------------|------------------------------------|----------------------------------|---------|----------------|---------------|----------------------------|-----------------------------------|-----------------------------------|----------------------|--|------------------|
| | | | | | | | | | Cross Slope- Driveway (%) 12% Max | Sidewalk X-Slope (%) | ADA - 4' Min. Path with 2% X-Slope (Y/N) | |
| 1 | 1.1 | ABC AND J, LLC | Waterloo Station Shopping Center | 17+00 | RT | 1 | 69.6 | Asphalt | 6.14% | 3.02% | N | Width > 40' |
| 2 | 2.1 | FROST PROPERTIES, LLP | J&D Handyman | 22+00 | RT | 1 | 43.9 | Asphalt | 8.29% | 2.98% | N | Width > 40' |
| | 2.2 | | Virginia Auto Group | 23+25 | RT | 2 | 47.5 | Asphalt | 5.31% | 0.67% | Y | Width > 40' |
| | 2.3 | | Good Year | 24+00 | RT | 3 | 38.2 | Asphalt | 4.47% | 1.02% | Y | |
| | 2.4 | | Warrenton Tire & Auto | 24+50 | RT | 4 | 38.0 | Asphalt | 5.43% | 1.89% | Y | |
| | 2.5 | | Napa Auto Parts | 25+00 | RT | 5 | 38.2 | Asphalt | 8.74% | 3.61% | N | |
| 3 | 3.1 | FROST FAMILY, LLC | Smith Brothers & Fast Signs | 26+25 | RT | 1 | 41.4 | Asphalt | 7.64% | 2.98% | N | Width > 40' |
| | 3.2 | | Frost Diner | 27+75 | RT | 2 | 48.7 | Asphalt | 8.37% | 1.09% | Y | Width > 40' |
| 4 | 4.1 | THIRD GENERATION, LLC | Shell Gas Station | 29+25 | RT | 1 | 47.1 | Concrete | 9.27% | 4.39% | N | Width > 40' |
| 5 | 5.1 | SPIRIT MASTER FUNDING III, LLC | Pizza Hut | 30+87 | RT | 1 | 28.8 | Asphalt | 4.94% | 6.12% | N | |
| 6 | 6.1 | KATHRYN H. MUDLOFT TRUSTEE | Private Drive | 32+12 | RT | 1 | 23.2 | Asphalt | N/A | N/A | N/A | Private Driveway |
| 7 | 7.1 | SHANTAM INVESTMENT INC | Sunco Gas Station | 33+25 | RT | 1 | 43.4 | Asphalt | 1.83% | 3.97% | N | Width > 40' |
| 8 | 8.1 | BTRZ, LLC | AAMCO Transmissions | 34+50 | RT | 1 | 43.4 | Asphalt | 1.85% | 1.83% | Y | Width > 40' |
| 9 | 9.1 | FIRST STATES INVESTORS HFS, L.P. | Wells Fargo Bank | 35+41 | RT | 1 | 35.6 | Concrete | 0.10% | 2.93% | N | |
| | 9.2 | | | 37+00 | RT | 2 | 49.1 | Concrete | 7.54% | 1.76% | Y | Width > 40' |
| 10 | 10.1 | ARTHUR E. & BARBARA MCKENNY | Massage Clinic & Jeweler | 37+87 | RT | 1 | 29.0 | Concrete | 1.95% | 1.27% | Y | |
| 11 | 11.1 | ROBERT F. KUBE | R.F. Kube Real Estate Appraiser | 41+00 | RT | 1 | 36.4 | Asphalt | 6.50% | 1.14% | Y | |
| 12 | 12.1 | 201 BROADVIEW, llc | Ship on Site | 41+00 | RT | 1 | 36.4 | Asphalt | 6.50% | 1.14% | Y | |
| 13 | 13.1 | BROADVIEW 211, LLC | Madison Taylor Real Estate | 42+33 | RT | 1 | 37.1 | Asphalt | 5.75% | 9.48% | N | |
| 14 | 14.1 | WELCO ASSOCIATES | Cash Point | 43+00 | RT | 1 | 35.4 | Asphalt | 6.02% | 6.27% | N | |
| 15 | 15.1 | DUCKWORTH & TUCKER MINTER | Moser Funeral Home | 44+00 | RT | 1 | 40.3 | Concrete | 0.41% | 0.05% | Y | Width > 40' |
| | 15.2 | | | 44+65 | RT | 2 | 24.4 | Concrete | 2.47% | 1.60% | Y | |
| 16 | 16.1 | REALTY INCOME CORPORATION | Midas | 45+39 | RT | 1 | 35.1 | Concrete | 2.61% | 8.67% | N | |
| 17 | 17.1 | DBLD, LLC | Rexel - Appliances | 46+00 | RT | 1 | 27.6 | Asphalt | 8.43% | 4.22% | N | |
| 18 | 18.1 | ROBERT & FRANK FOLEY | L.J. Foley Inc. | 46+75 | RT | 1 | 50.0 | Asphalt | 6.02% | 2.35% | N | Width > 40' |
| | 18.2 | | | 47+34 | RT | 2 | 26.4 | Asphalt | 12.05% | 6.71% | N | |
| 19 | 19.1 | R & L FOODS, LLC | Wendy's | 48+65 | RT | 1 | 29.7 | Asphalt | 4.51% | 3.75% | N | |
| | 19.2 | | | 49+50 | RT | 2 | 30.0 | Asphalt | 5.11% | 2.85% | N | |
| 20 | 20.1 | CECIL'S SERVICE AND EQUIPMENT, INC | Trusted Auto Care | 50+25 | RT | 1 | 42.0 | Asphalt | 12.41% | 1.81% | N | Width > 40' |
| 21 | 21.1 | BROADVIEW PROPERTY, LLC | Shell Gas Station | 51+00 | RT | 1 | 49.0 | Asphalt | 9.79% | 1.86% | Y | Width > 40' |
| | 21.2 | | | 52+25 | RT | 2 | 47.9 | Asphalt | 6.62% | 0.40% | Y | Width > 40' |
| | 21.3 | | | 53+00 | RT | 3 | 33.1 | Asphalt | 3.55% | 1.93% | Y | |
| 22 | 22.1 | DACDAB, LLC | Wilson Automotive | 53+75 | RT | 1 | 48.2 | Concrete | 0.78% | 0.83% | Y | Width > 40' |
| | 22.2 | | | 55+00 | RT | 2 | 48.7 | Concrete | 7.08% | 1.94% | Y | Width > 40' |
| 23 | 23.1 | RHC ASSPCIATES | McDonald's | 55+75 | RT | 1 | 34.7 | Concrete | 2.58% | 4.05% | N | |
| | 23.2 | | | 56+75 | RT | 2 | 33.9 | Concrete | 0.42% | 4.45% | N | |
| 24 | 24.1 | FRANCHISE REALTY INTERSTATE CORP | McDonald's Additional Parking | 57+50 | RT | 1 | 30.7 | Concrete | 7.02% | 1.00% | Y | |
| 25 | 25.1 | PIEDMONT LIMITED PARTNERSHIP | Dunkin' Donuts | 62+50 | RT | 1 | 47.6 | Concrete | 8.54% | 8.54% | N | Width > 40' |
| 26 | 26.1 | ROBERT M. & BRIDGET H. MCCLANAHAN | McClanahan's Camera | 64+50 | RT | 1 | 28.6 | Asphalt | 8.50% | 6.15% | N | |
| | 26.2 | | Warrenton Galleria | 65+00 | RT | 2 | 48.3 | Asphalt | 4.32% | 6.05% | N | Width > 40' |
| 27 | 27.1 | WA- Warren Realty Associates, LLC | WaWa | 17+50 | LT | 1 | 45.0 | Concrete | 5.20% | 6.77% | N | Width > 40' |
| | 27.2 | | | 18+95 | LT | 2 | 49.4 | Concrete | 2.74% | 1.97% | N | Width > 40' |
| 28 | 28.1 | MIGHTY MIDGET, LLC | Foster's Grill | 23+50 | LT | 1 | 48.2 | Asphalt | 6.41% | 9.88% | N | Width > 40' |

ADA/Bicycle Compliance Study - Existing Commercial Entrances Evaluation

Note: Standard Single Two-Way Entrance Width Should be 30'-40' According to VDOT Road Design Manual

* Asphalt entrances include concrete gutter pan and approx. 2' concrete strip @ roadway edge

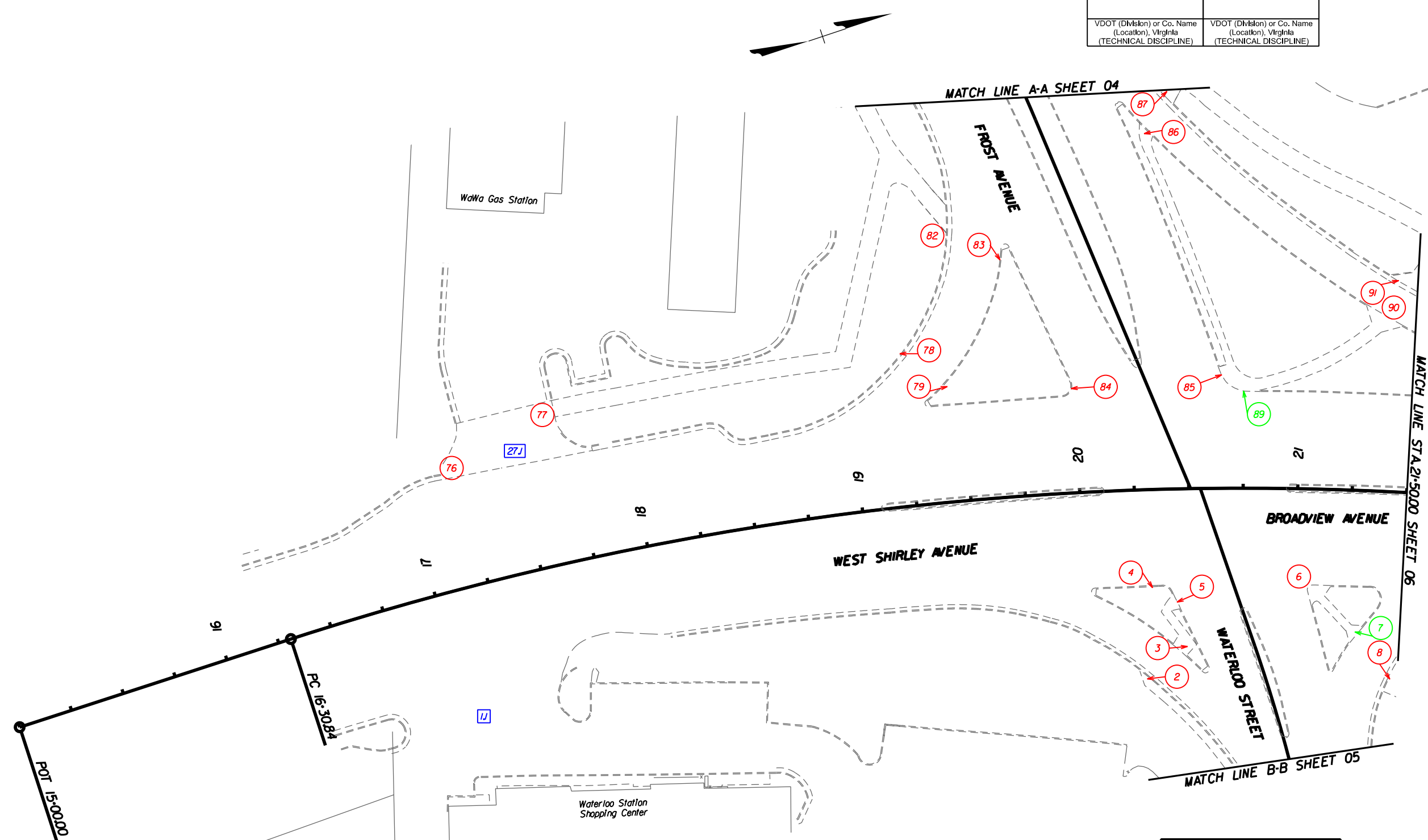
| Property # | Driveway ID | Property Owner | Business Name | Station | Offset (LT/RT) | Entrances (#) | Ex. Entrance Width(s) (FT) | Entrance Type (Asphalt*/Concrete) | Edge of Pavement | Pedestrian Walkway | | Remarks |
|------------|-------------|---|--------------------------|------------|----------------|---------------|----------------------------|-----------------------------------|-----------------------------------|----------------------|--|-------------|
| | | | | | | | | | Cross Slope- Driveway (%) 12% Max | Sidewalk X-Slope (%) | ADA - 4' Min. Path with 2% X-Slope (Y/N) | |
| 29 | 29.1 | CAROLINA DE KROES AND BART NARINUS | Burger King | 24+50 | LT | 1 | 39.9 | Concrete | 1.32% | 2.78% | N | |
| | 29.2 | DE KROES | | 25+50 | LT | 2 | 40.2 | Concrete | 6.65% | 2.77% | N | Width > 40' |
| 30 | 30.1 | WFC, LLC | J.K. Auto Parts | 26+75 | LT | 1 | 48.3 | Concrete | 5.84% | 1.59% | Y | Width > 40' |
| | 30.2 | | Wells Auto Sales | 28+25 | LT | 2 | 49.0 | Concrete | 8.40% | 0.38% | Y | Width > 40' |
| 31 | 31.1 | CHU'S ENTERPRISE, INC | El Toro | 29+37 | LT | 1 | 30.0 | Asphalt | 8.56% | 0.78% | Y | |
| | 31.2 | | El Toro | 30+25 | LT | 2 | 30.9 | Asphalt | 3.82% | 6.90% | N | |
| 32 | 32.1 | BAJRANG BALI, INC | Jefferson Motel | 31+00 | LT | 1 | 46.4 | Asphalt | 5.94% | 5.09% | N | Width > 40' |
| 33 | 33.1 | SALEM, LLC | Subway | 31+75 | LT | 2 | 22.8 | Asphalt | 8.35% | 7.44% | N | |
| 34 | 34.1 | OAK VIEW NATIONAL BANK | Oak View National Bank | 33+00 | LT | 1 | 37.9 | Asphalt | 5.59% | 5.14% | N | |
| | 34.2 | | | 34+75 | LT | 2 | 42.2 | Asphalt | 6.18% | 2.89% | N | Width > 40' |
| 35 | 35.1 | BROADVIEW, LLC | Mattress Authority | 35+50 | LT | 1 | 47.3 | Asphalt | 8.85% | 12.98% | N | Width > 40' |
| | 35.2 | | Sherwin Williams | 37+00 | LT | 2 | 48.2 | Asphalt | 7.09% | 3.09% | N | Width > 40' |
| 36 | 36.1 | CAMPBELL TRUSTEE | STIHL | 37+00 | LT | 1 | 48.2 | Asphalt | 7.09% | 3.09% | N | Width > 40' |
| | 36.2 | | | 37+65 | LT | 2 | 22.8 | Asphalt | 6.51% | 5.14% | N | |
| 37 | 37.1 | WEN'S INTERNATIONAL CORP | Rip Van Winkle Motel | 39+86 | LT | 1 | 27.6 | Asphalt | 5.11% | 2.35% | N | |
| 38 | 38.1 | KFC U.S. PROPERTIES, INC | KFC & Long John Silver's | 41+00 | LT | 1 | 42.4 | Concrete | 3.81% | 0.70% | Y | Width > 40' |
| 39 | 39.1 | UNION FIRST MARKET BANK | Union Bank | 41+89 | LT | 1 | 26.3 | Asphalt | 4.38% | 9.90% | N | |
| | 39.2 | | | 43+13 | LT | 2 | 21.5 | Asphalt | 3.48% | 6.48% | N | |
| 40 | 40.1 | SHANTAM TECH INC | Taco Bell | 44+25 | LT | 1 | 48.1 | Concrete | 2.44% | 2.12% | N | Width > 40' |
| | 40.2 | | Vacant | 45+75 | LT | 2 | 49.5 | Concrete | 1.73% | 3.26% | N | Width > 40' |
| 41 | 41.1 | FLETCHER CENTRE PROPERTIES, L.C. | Warrenton Plaza | 47+34 | LT | 1 | 32.5 | Asphalt | 3.76% | 1.94% | Y | |
| | 41.2 | | | 48+88 | LT | 2 | 31.0 | Asphalt | 8.52% | 4.49% | N | |
| | 41.3 | | | ACE Motors | 50+00 | LT | 3 | 26.7 | Asphalt | 8.17% | 5.45% | N |
| 42 | 42.1 | GIANCARLO AND MATILDE ODERDA | Jiffy Lube | 51+14 | LT | 1 | 33.2 | Concrete | 5.11% | 0.39% | Y | |
| 43 | 43.1 | 310 BROADVIEW ENTERPRISES, LLC | Mattress Warehouse | 52+25 | LT | 1 | 30.8 | Concrete | 7.61% | 2.43% | N | |
| 44 | 44.1 | LEONORA R. BERNHEISEL TRUSTEE | Piedmont Motors | 53+00 | LT | 1 | 34.1 | Concrete | 1.85% | 2.02% | Y | |
| | 44.2 | | | 54+00 | LT | 2 | 47.3 | Concrete | 1.42% | 0.85% | Y | Width > 40' |
| 45 | 45.1 | CHICK'S SERVICE, INC | Gulf Gas | 55+00 | LT | 1 | 43.8 | Concrete | 5.99% | 6.51% | N | Width > 40' |
| | 45.2 | | | 56+00 | LT | 2 | 45.0 | Concrete | 7.31% | 7.48% | N | Width > 40' |
| 46 | 46.1 | POTA'S PROPERTIES LIMITED PARTNERSHIP, RLLP | Red Hot & Blue BBQ Grill | 56+91 | LT | 1 | 48.4 | Asphalt | 6.08% | 7.23% | N | Width > 40' |
| | 46.2 | | | 58+00 | LT | 2 | 48.3 | Asphalt | 9.78% | 3.62% | N | Width > 40' |
| 47 | 47.1 | FOG HOLDINGD, LLC | McMahon's Restaurant | 59+60 | LT | 1 | 47.9 | Asphalt | 8.55% | 4.96% | N | Width > 40' |
| 48 | 48.1 | SOUTHSIDE OIL, LLC | Exxon Mobil Gas Station | 64+50 | LT | 1 | 41.3 | Concrete | 6.25% | 6.09% | N | Width > 40' |

APPENDIX E
ADA/ Bicycle Compliancy Study –
Location Plans – Critical Improvements

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (D|str|ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>
DESIGN BY <Designer_Name (0001.000-0000) (D|str|ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

| | | | | | |
|---|-------|-------|---|---------|-----------|
| REVISED | STATE | ROUTE | STATE | PROJECT | SHEET NO. |
| | VA. | | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |

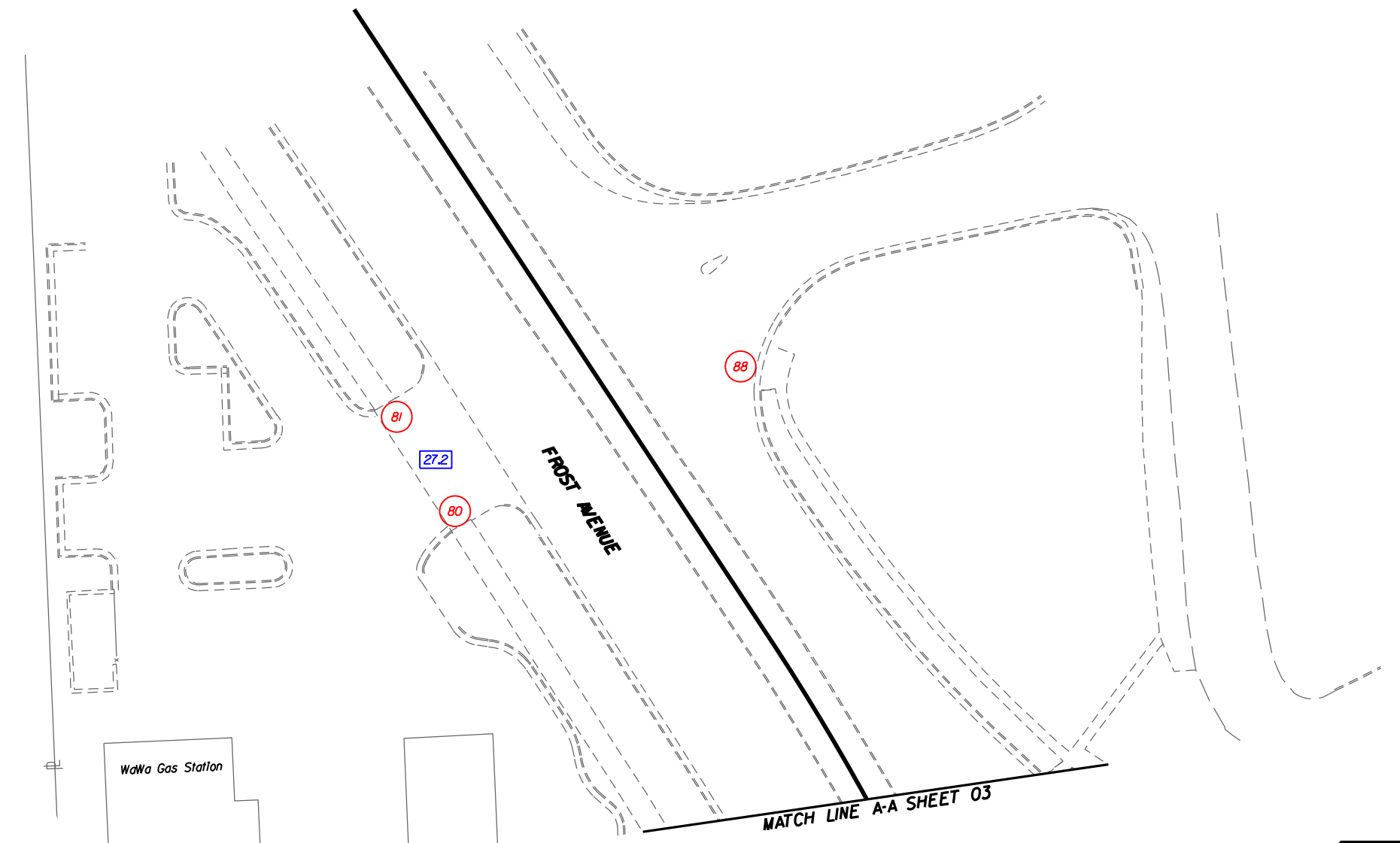
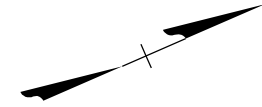


| | | |
|---|--|--|
| <p>LEGEND</p> <p> Sidewalk Upgrades Curb Ramp to Remain Curb Ramp Improvements Entrance to Remain Entrance Improvements </p> | | <p>REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)</p> <p>Mainline Profile 3A Drainage Descr.</p> |
| <p>SCALE</p> <p>0 25' 50'</p> | | <p>PROJECT</p> <p>SHEET NO. 03</p> |

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (District)> _____
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (District)> _____
DESIGN BY <Designer_Name (0001.000-0000) (District)> _____
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (District)> _____

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | STATE | | SHEET NO. |
| | ROUTE | PROJECT | | |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | |



| REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.) | |
|--|----|
| Mainline Profile | 3A |
| Drainage Descr. | |

| LEGEND | | | |
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| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

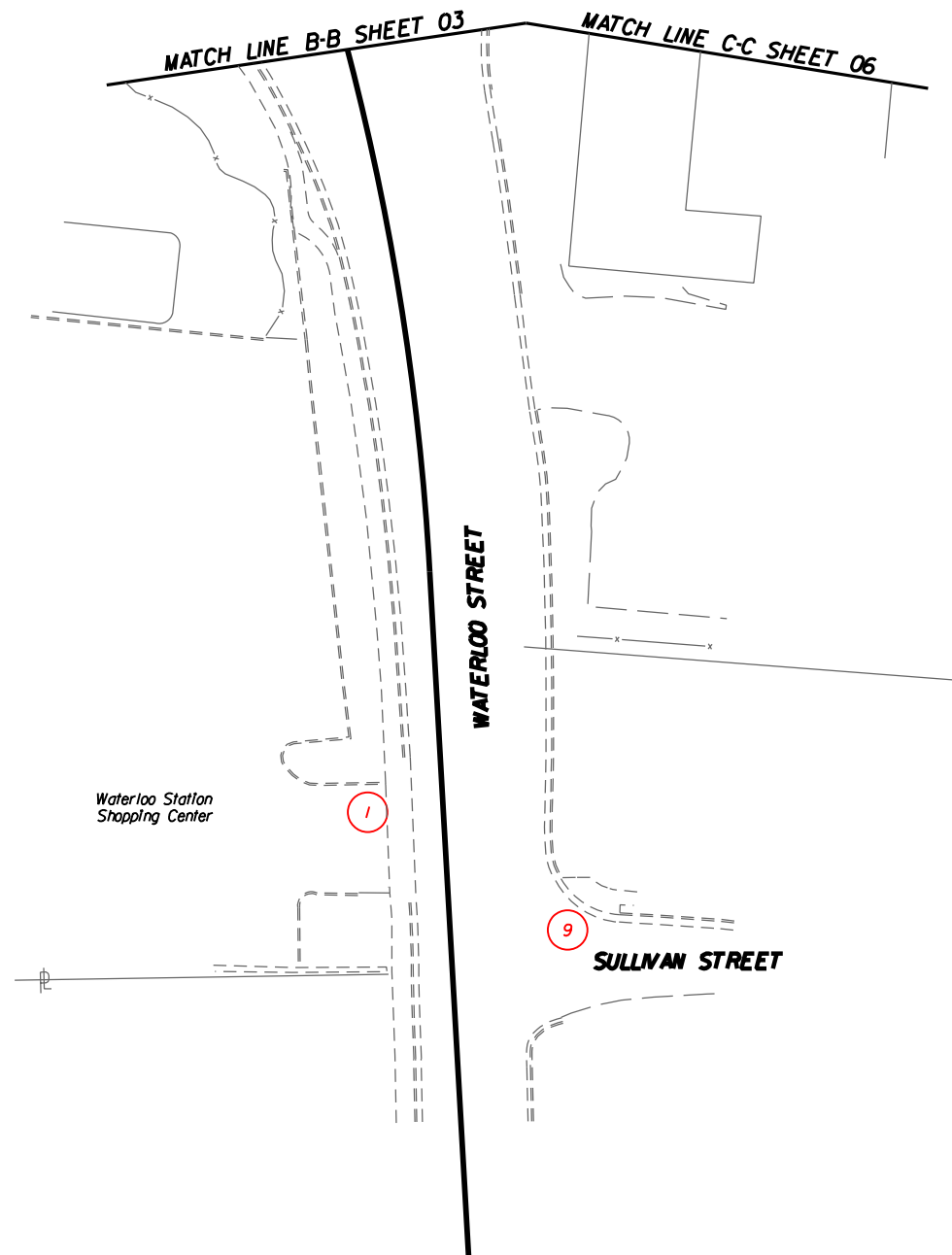
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| SCALE 0 25' 50' | PROJECT | SHEET NO. 04 |
|--------------------|---------|------------------------|

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (District)> _____
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (District)> _____
DESIGN BY <Designer_Name (0001.000-0000) (District)> _____
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (District)> _____

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS



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| REVISED | STATE | STATE | | SHEET NO. |
| | ROUTE | PROJECT | | |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |



REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

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| Mainline Profile | 3A |
| Drainage Descr. | |

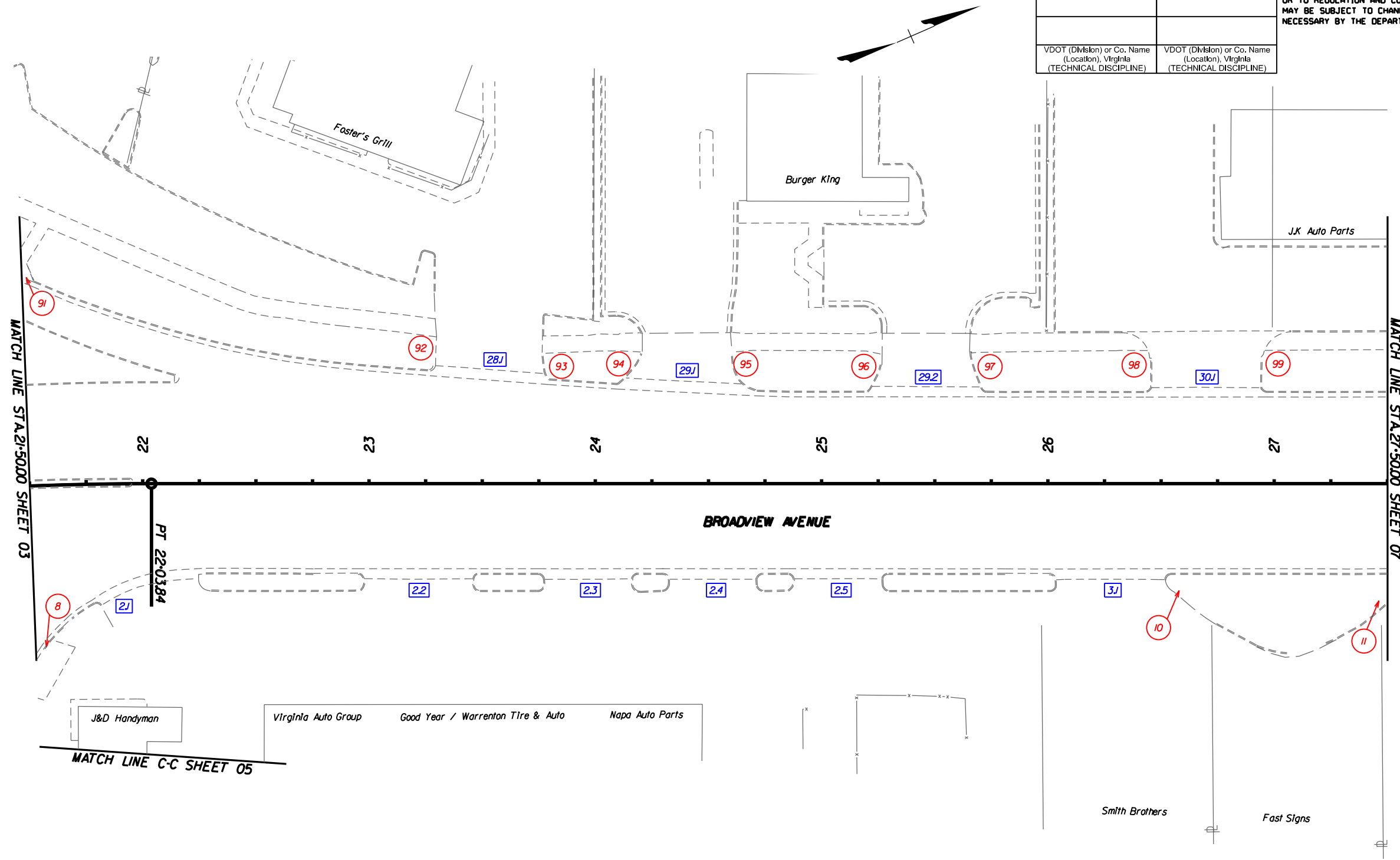
| LEGEND | | | |
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| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

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| SCALE 0 25' 50' | PROJECT | SHEET NO. 05 |
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PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (District)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (District)>
DESIGN BY <Designer_Name (0001.000-0000) (District)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (District)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |



| REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.) | |
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| Mainline Profile | 3A |
| Drainage Descr. | |

| LEGEND | | | |
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| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

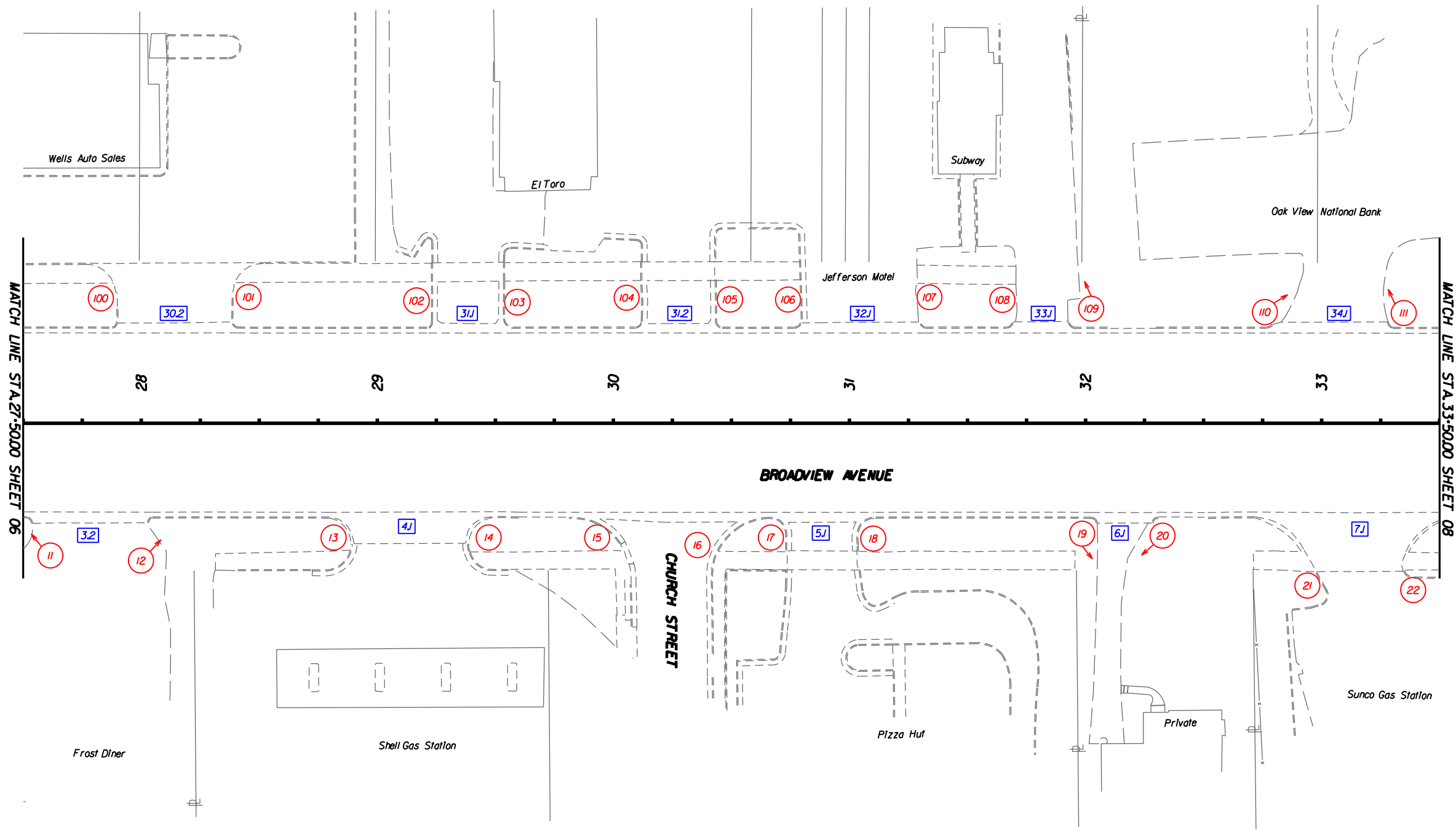
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| SCALE 0 25' 50' | PROJECT | SHEET NO. 06 |
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PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (D1str/ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (D1str/ct)>
DESIGN BY <Designer_Name (0001.000-0000) (D1str/ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (D1str/ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS



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| REVISED | STATE | STATE | SHEET NO. |
| | ROUTE | PROJECT | |
| | VA. | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | |



REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Mainline Profile 3A
Drainage Descr.

| LEGEND | | | |
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| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

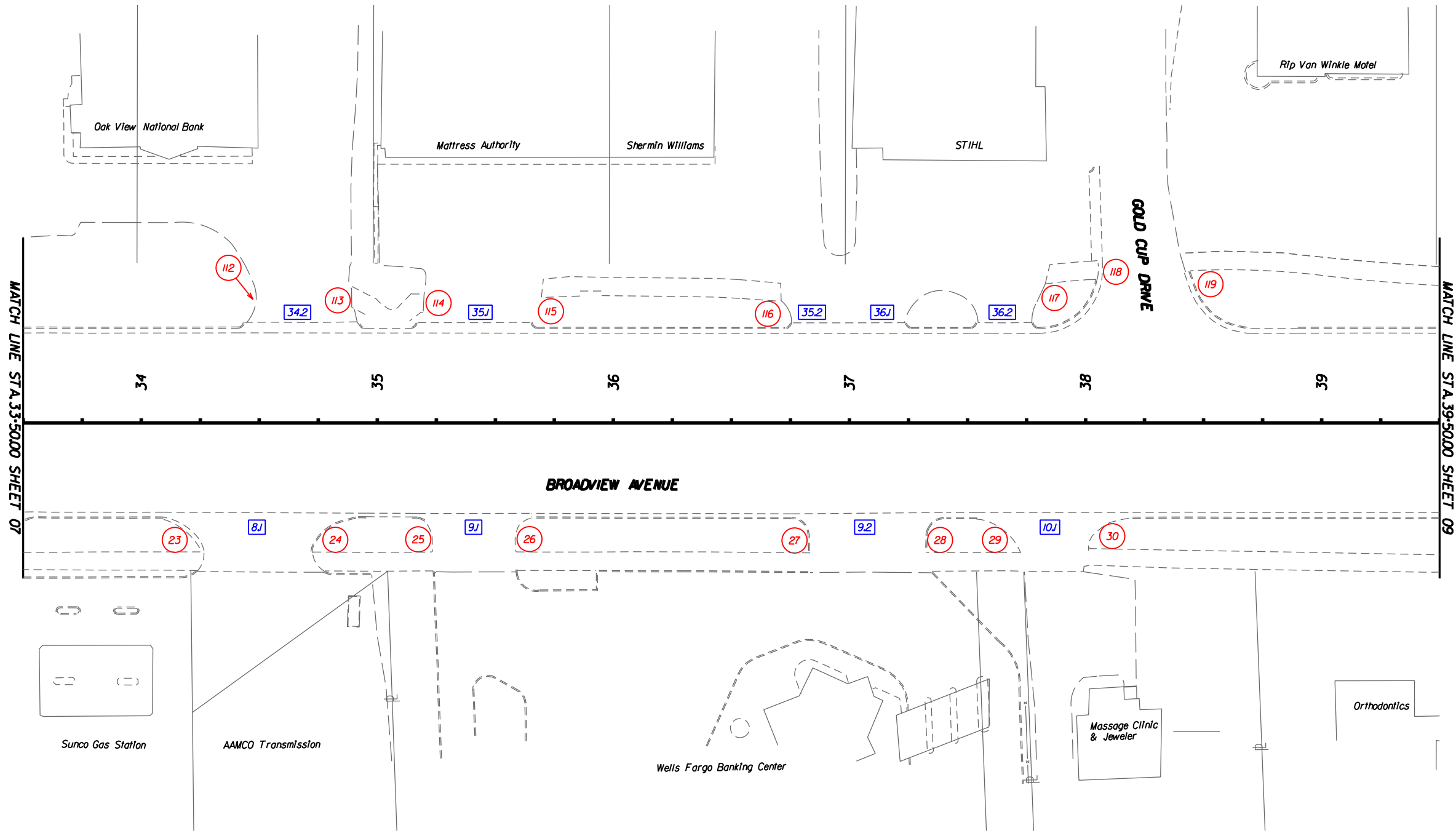
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| SCALE 0 25' 50' | PROJECT | SHEET NO. 07 |
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PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (D1str/ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (D1str/ct)>
DESIGN BY <Designer_Name (0001.000-0000) (D1str/ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (D1str/ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS



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| REVISED | STATE | STATE | SHEET NO. |
| | ROUTE | PROJECT | |
| | VA. | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | |



REFERENCES
(PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

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| Mainline Profile | 3A |
| Drainage Descr. | |

LEGEND

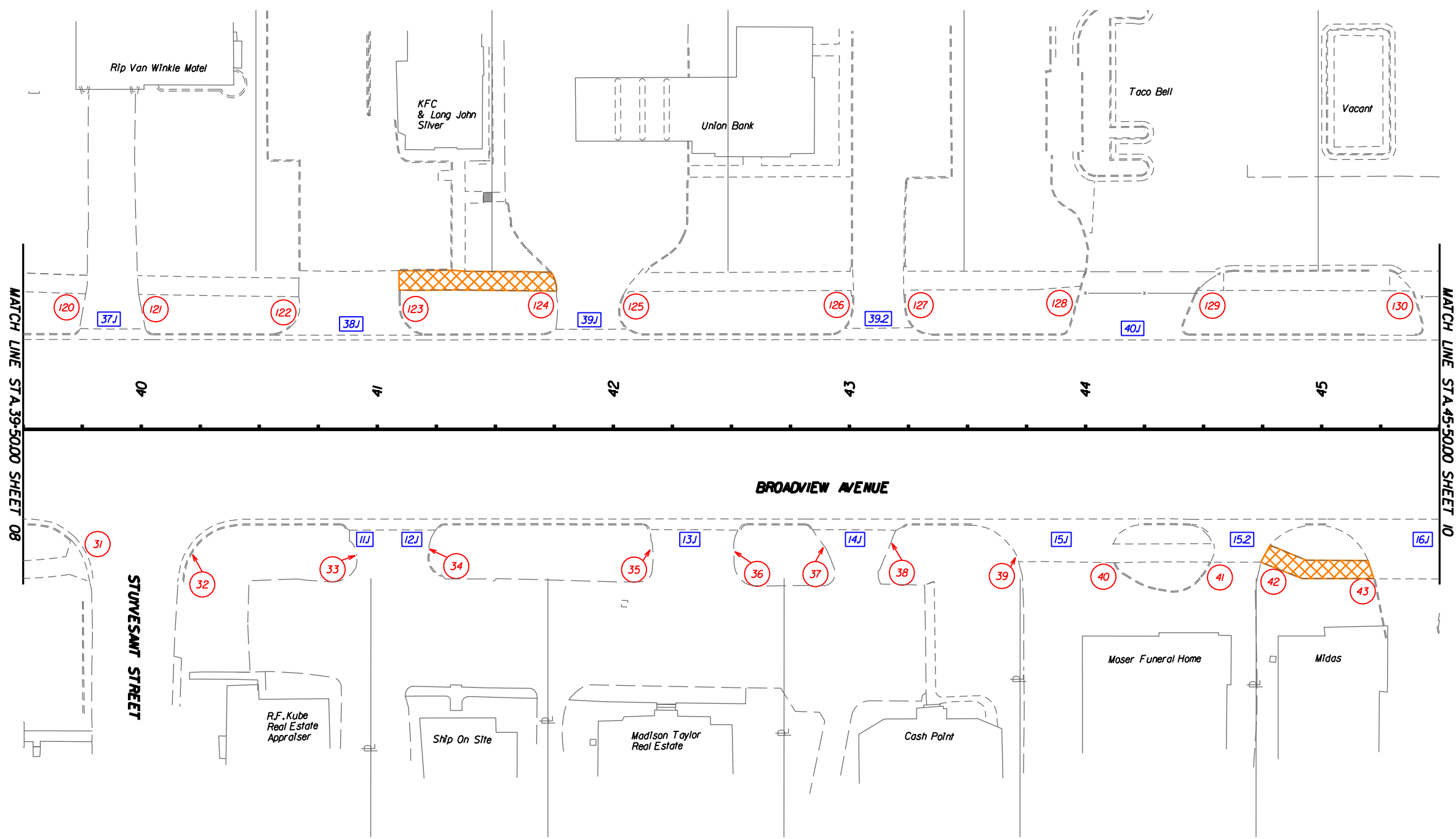
| | | |
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| Sidewalk Upgrades | Curb Ramp to Remain | Curb Ramp Improvements |
| Entrance to Remain | Entrance Improvements | |

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| SCALE 0 25' 50' | PROJECT | SHEET NO. 08 |
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PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (D|str|ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>
DESIGN BY <Designer_Name (0001.000-0000) (D|str|ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |



REFERENCES
(PROFILES, DETAIL & DRAINAGE
DESCRIPTION SHEETS, ETC.)

Mainline Profile 3A
Drainage Descr.

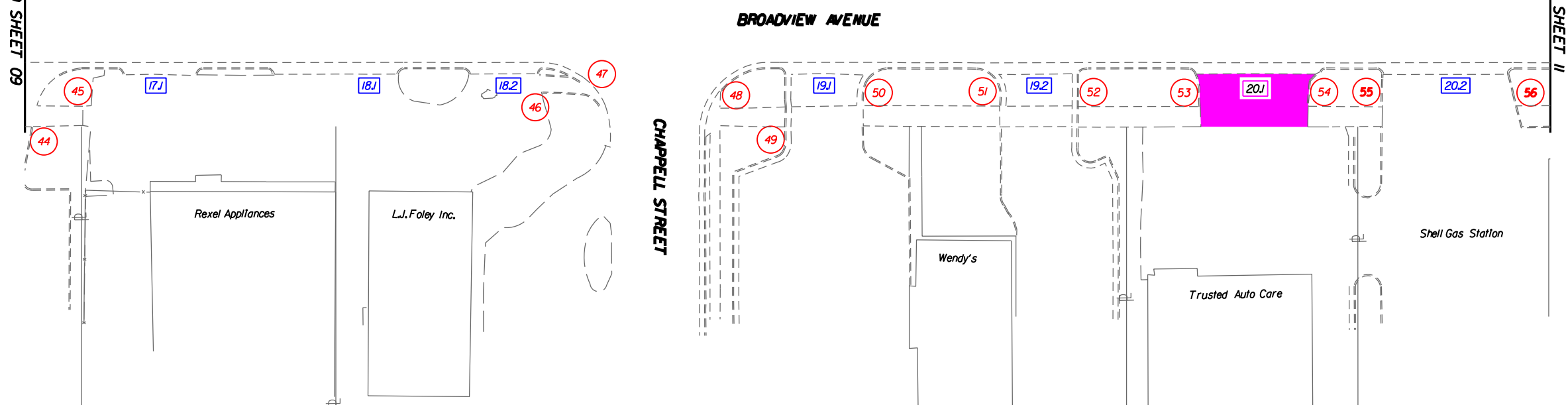
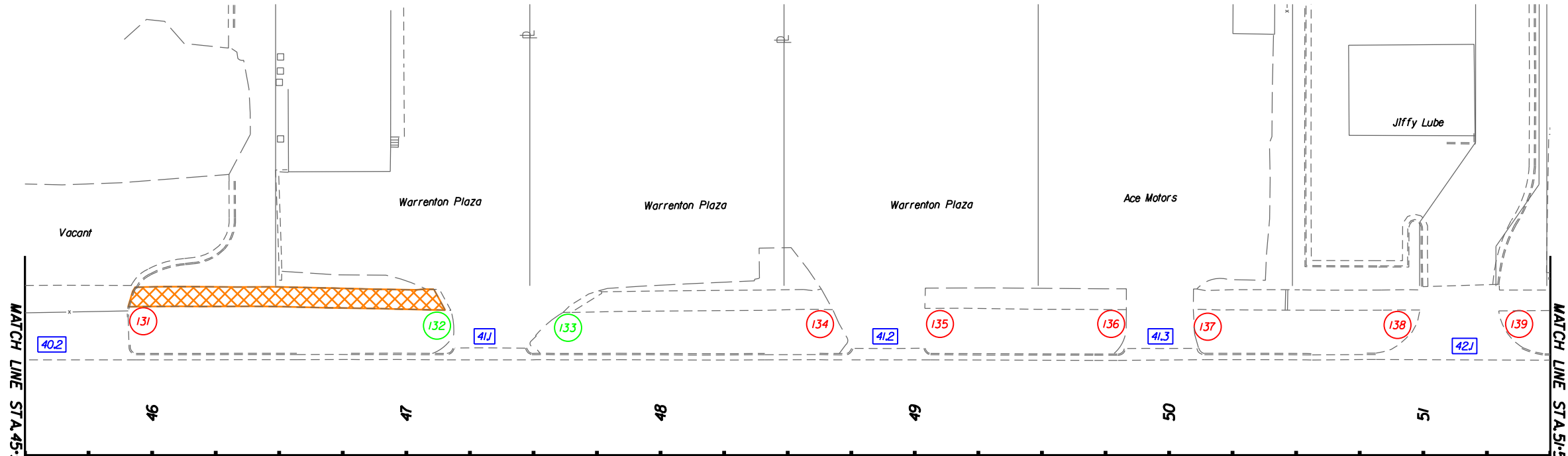
| LEGEND | | | |
|--------|------------------------|--|---------------------|
| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | Curb Ramp Improvements | | Entrance to Remain |
| | Entrance Improvements | | |

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| SCALE 0 25' 50' | PROJECT | SHEET NO. 09 |
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PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (D|str|ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>
DESIGN BY <Designer_Name (0001.000-0000) (D|str|ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (D|str|ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |



REFERENCES
(PROFILES, DETAIL & DRAINAGE
DESCRIPTION SHEETS, ETC.)

Mainline Profile 3A
Drainage Descr.

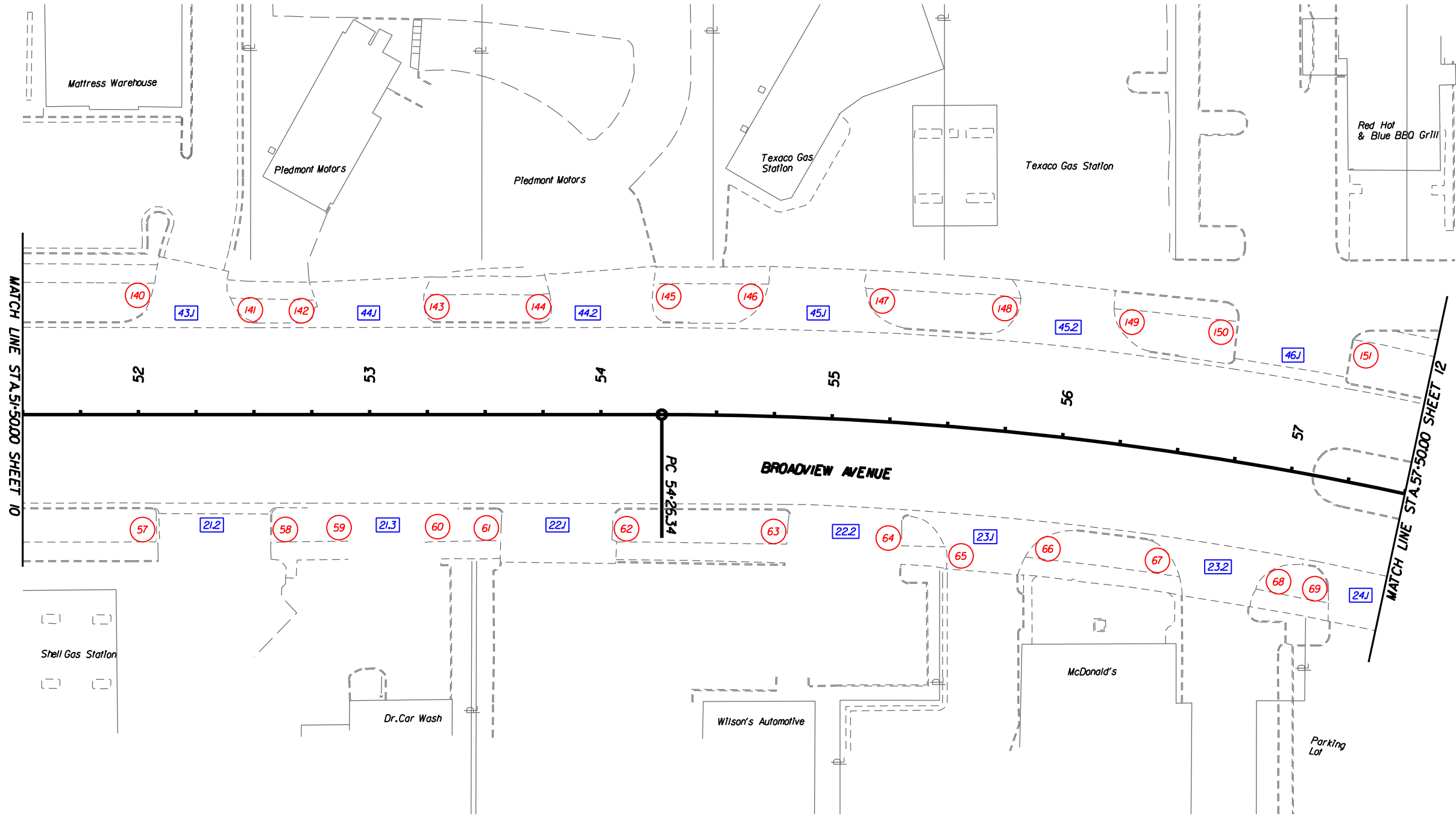
| LEGEND | | | |
|--------|--------------------|--|------------------------|
| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

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| SCALE | PROJECT | SHEET NO. |
| 0 25' 50' | | 10 |

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (Distr/ct)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (Distr/ct)>
DESIGN BY <Designer_Name (0001.000-0000) (Distr/ct)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (Distr/ct)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | VDOT (Division or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE)) | | |



| REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.) | |
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| Mainline Profile | 3A |
| Drainage Descr. | |

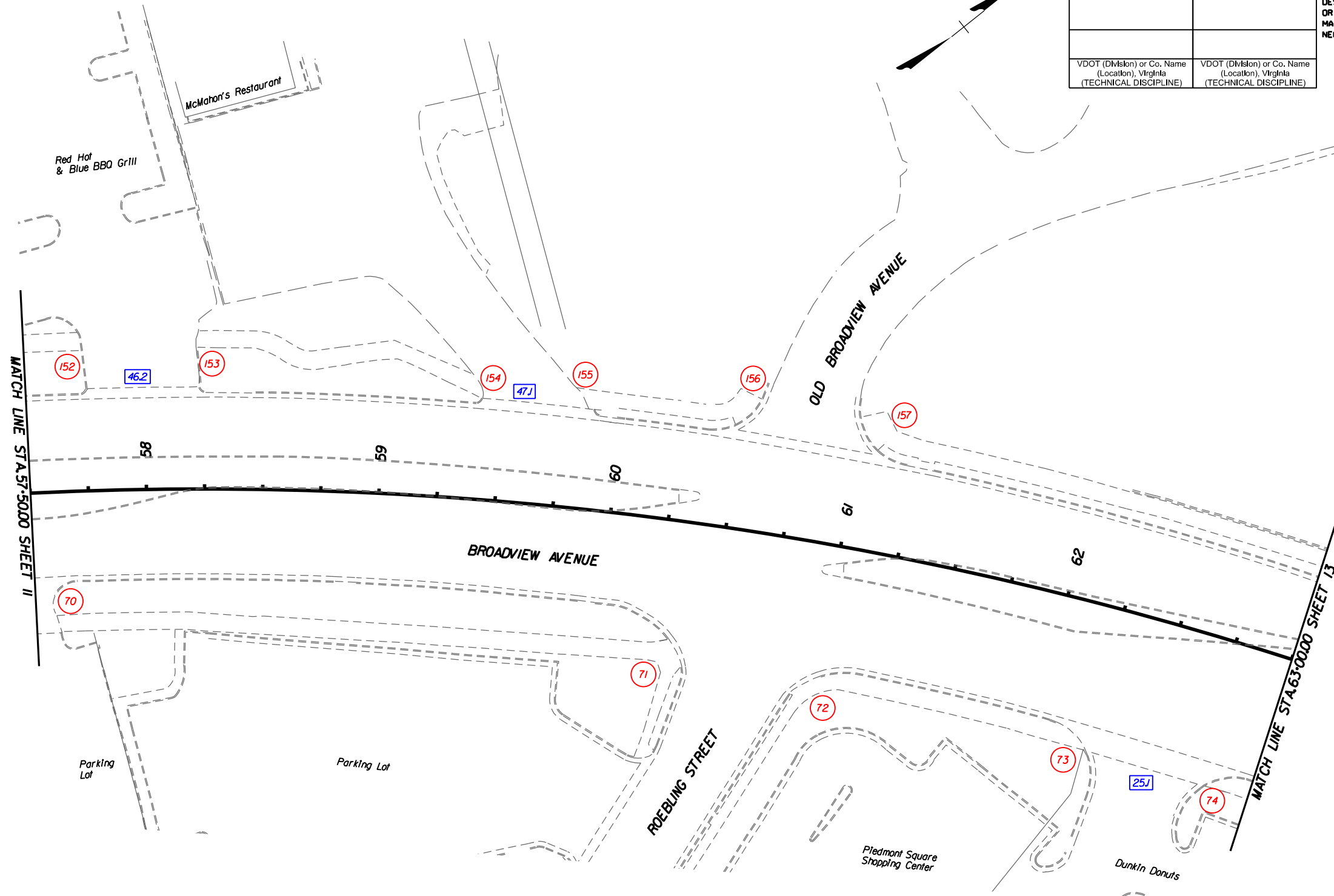
| LEGEND | | | |
|--------|--------------------|--|------------------------|
| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | | | Curb Ramp Improvements |
| | Entrance to Remain | | Entrance Improvements |

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| SCALE 0 25' 50' | PROJECT | SHEET NO. 11 |
|--------------------|---------|-----------------|

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (District)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (District)>
DESIGN BY <Designer_Name (0001.000-0000) (District)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (District)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | |



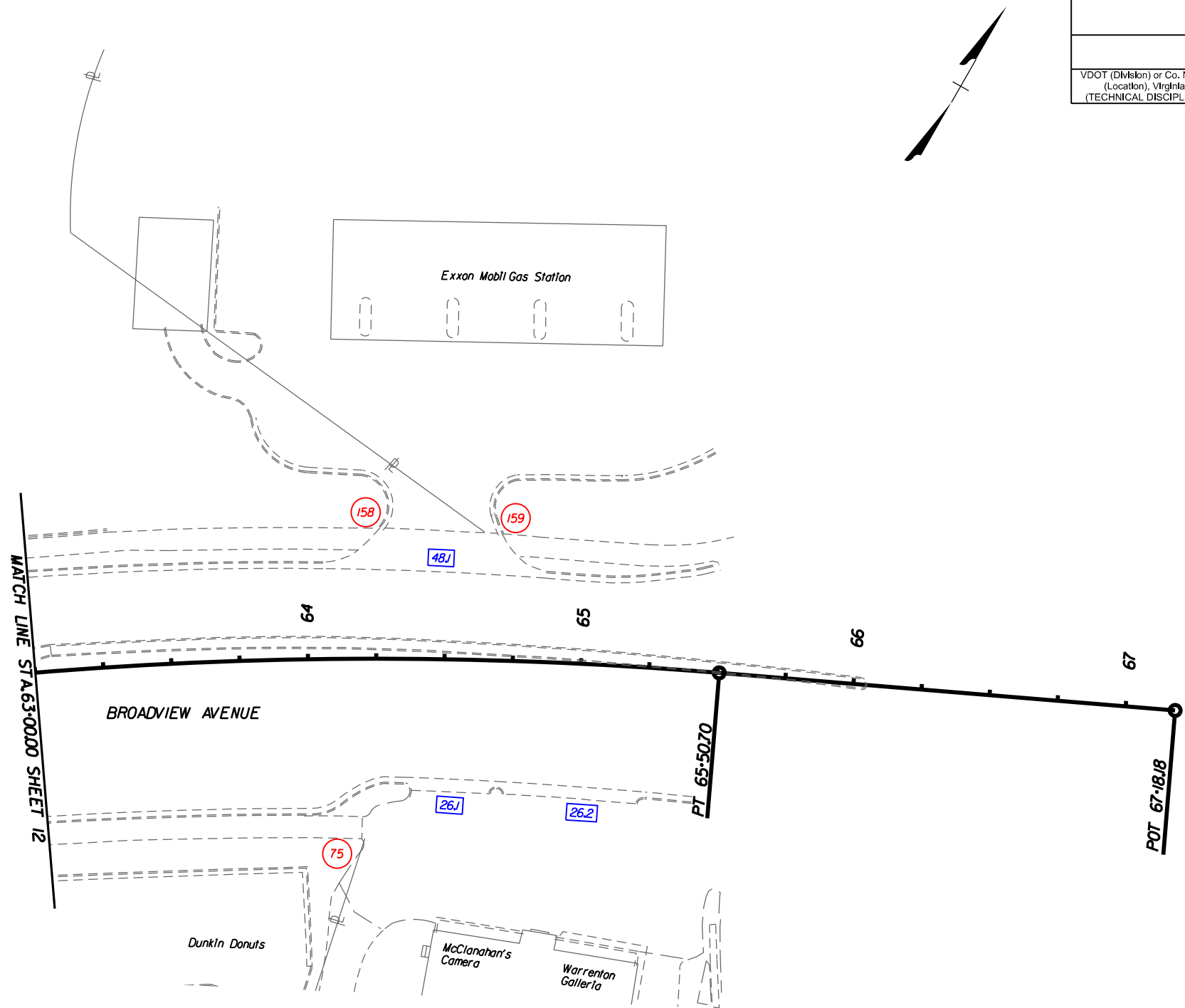
| LEGEND | |
|--------|------------------------|
| | Sidewalk Upgrades |
| | Curb Ramp to Remain |
| | Curb Ramp Improvements |
| | Entrance to Remain |
| | Entrance Improvements |

| REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.) | |
|--|----|
| Mainline Profile | 3A |
| Drainage Descr. | |

PROJECT MANAGER <Project_Mgr_Name (0001.000-0000) (District)>
SURVEYED BY, DATE <Surveyor_Name (0001.000-0000) (District)>
DESIGN BY <Designer_Name (0001.000-0000) (District)>
SUBSURFACE UTILITY BY, DATE <Surveyor_Name (0001.000-0000) (District)>

ADA-BICYCLE COMPLIANCE STUDY CRITICAL IMPROVEMENTS

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|---|-------|---|---------|-----------|
| REVISED | STATE | ROUTE | PROJECT | SHEET NO. |
| | VA. | | | |
| DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT | | | | |
| VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | VDOT (Division) or Co. Name (Location), Virginia (TECHNICAL DISCIPLINE) | | |



| LEGEND | | REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.) | |
|--------|-----------------------|--|------------------------|
| | Sidewalk Upgrades | | Curb Ramp to Remain |
| | Entrance to Remain | | Curb Ramp Improvements |
| | Entrance Improvements | | |
| | | Mainline Profile | 3A |
| | | Drainage Descr. | |